

Southern Queens Greenway

Segment Three: Cross Bay Boulevard to 114th Street

The Conduit Greenway ends and the Southern Queens Greenway begins at the intersection of 149th Avenue and Cross Bay Boulevard. The existing "greenstreet" traffic triangle on the east side of Cross Bay Boulevard should be reconstructed to provide a path for west-bound bicycle travel, and to serve as a gateway for the Southern Queens Greenway. This intersection should also be improved with the installation of a stop sign at Albert Road to calm the high speed traffic entering the intersection. New 5' bicycle lanes would continue on 149th Avenue to Centreville Avenue.

East of 149th Avenue, the route would travel along one of the two proposed routes. Alternative One travels south on Centreville Avenue, followed by Eckford and Hawtree Avenues (east bound) and Albert Road (west bound). With the exception of Centreville, these streets would be improved with 5' bicycle lanes. A connection is made with the Centreville Park and Playground located at Centreville and Albert Roads.

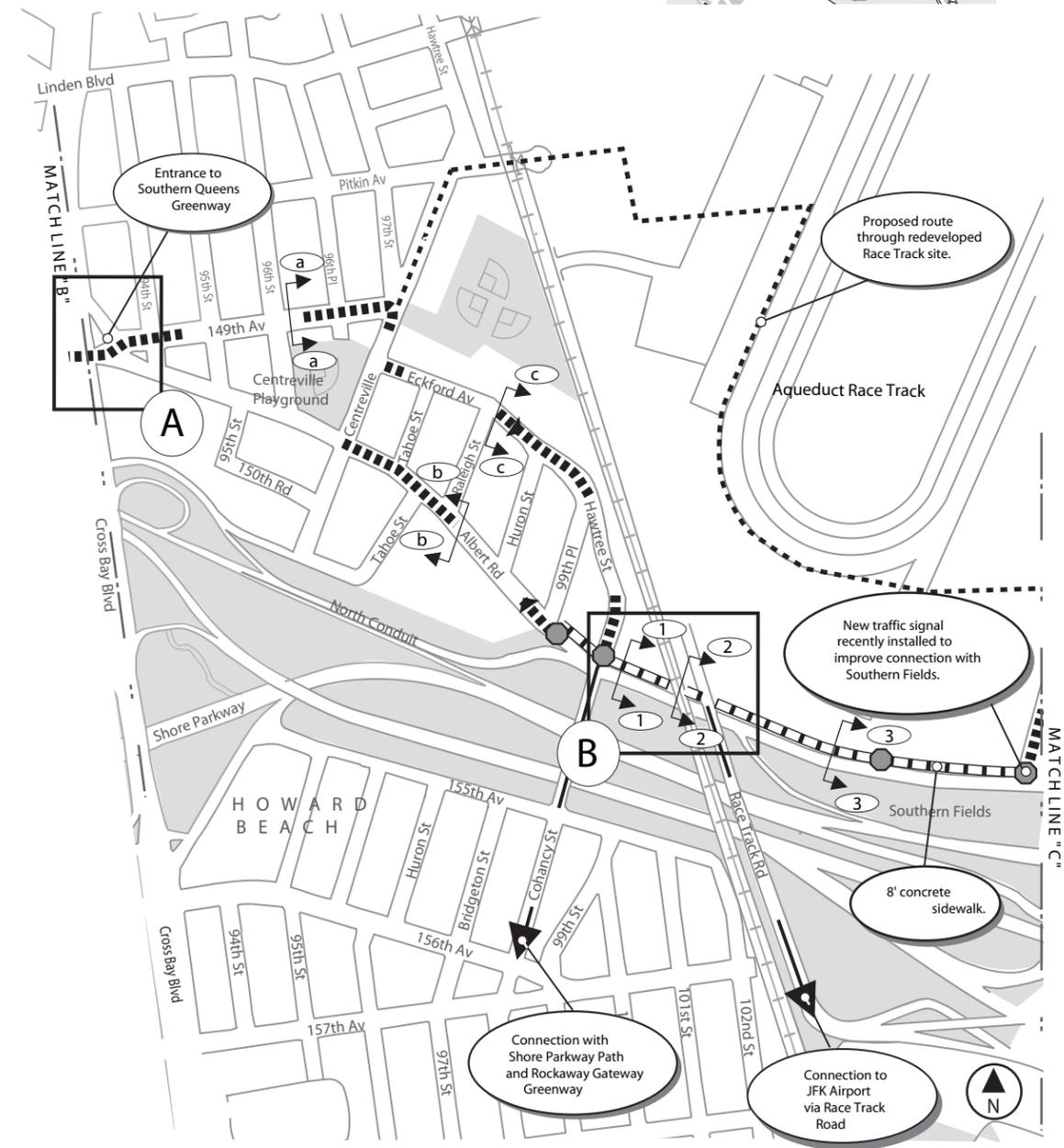
At Hawtree Road and North Conduit, Alternative One continues along a new 13' concrete path adjacent to North Conduit and connects with and travels in front of the Aqueduct A train station. The proposed path, which will replace an existing 5' concrete sidewalk, will provide 8' for 2-way bicycle traffic and 5' for pedestrians. A fence will separate pedestrians from cyclists and a barrier rail will buffer cyclists from west-bound, North Conduit motor vehicle traffic. The pedestrian fence will also eliminate the current dangerous practice of station drop-offs along the high-speed North Conduit Avenue. The Greenway will connect with JFK Airport and the proposed Shore Parkway Greenway extension at both Cohancy Street and Race Track Road.

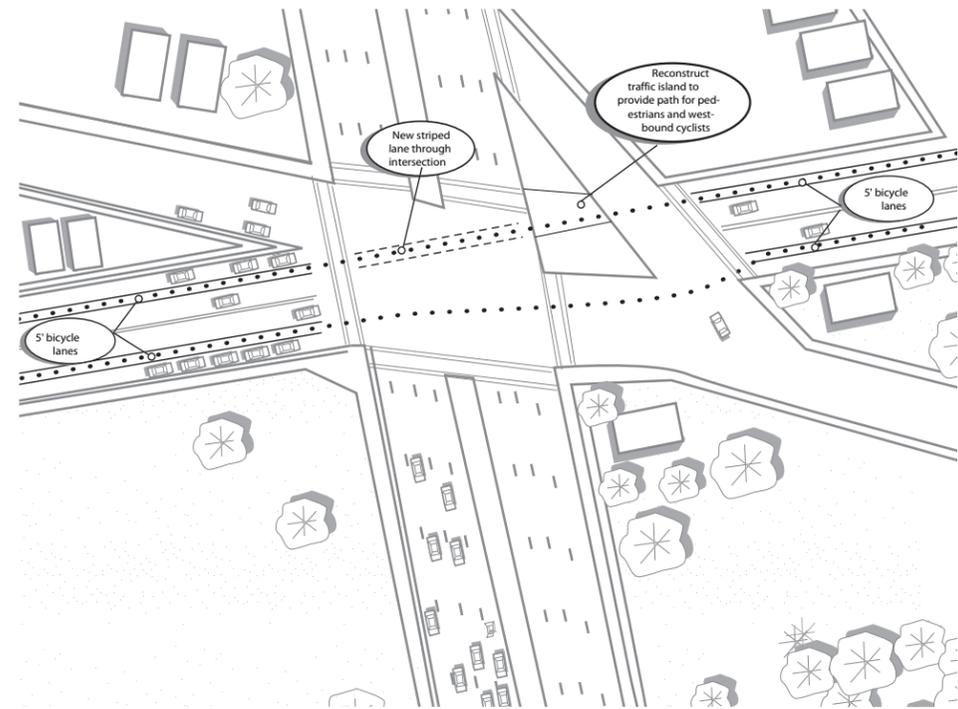
East of Race Track Road, Alternative One continues to travel on a reconstructed path to 114th Street. Within this section, the location of the Race Track Road bridge abutment and the presence of fire hydrants along Aqueduct Race Track sidewalk limit the path's width to 8'. A barrier rail, interrupted every 20' with trees, would be installed along North Conduit to buffer the Greenway from west-bound motor vehicle traffic. Because of the limited path width, and the former exclusive use of this space for pedestrians, greenway signs would instruct users to "go slow-respect others".

Alternative One was determined to be the most direct route which could be implemented prior to potential redevelopment of Aqueduct Race Track site. Currently, the race track site and the A train and Race Track Road bridges hinder east-west, non-motorized travel. Alternatives considered, such as routing cyclists south on Cross Bay or Cohancy and east on the sidewalk on South Conduit, or using the existing pedestrian underpass to the north of the A train Station, were deemed too indirect for the Greenway's intended east-west travel. Alternative Two should be pursued if redevelopment plans for Aqueduct Race Track progress. We urge the inclusion of a multi-use path in further planning for the development. Minimally, an on-street route could be planned according to the current conceptual design. From 149th Avenue, Alternative Two would travel north on Centreville, east on Pitkin Avenue and through the existing railroad underpass to the new community, ultimately connecting with Segment Four of the Southern Queens Greenway to the east.

Legend

- Proposed path
- - - Alternative One: Proposed route alignment along existing sidewalk
- ▬▬▬ Alternative One: Proposed on-street route
- · - · - Alternative Two
- Roadway Crossing
- Site Enlargement
- ⓐ Cross Section

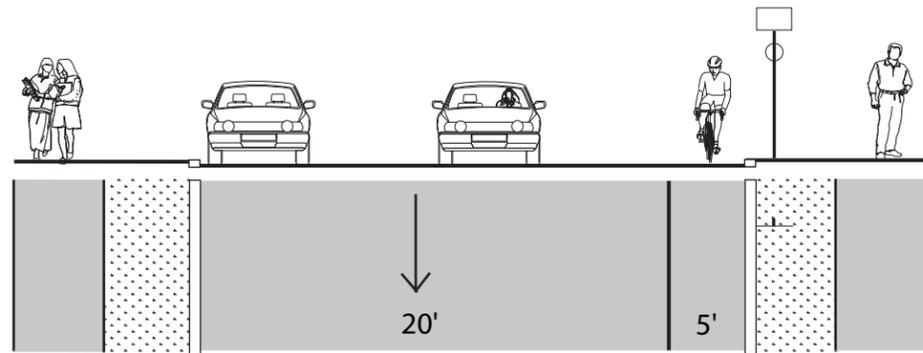




Cross Bay Boulevard at North Conduit

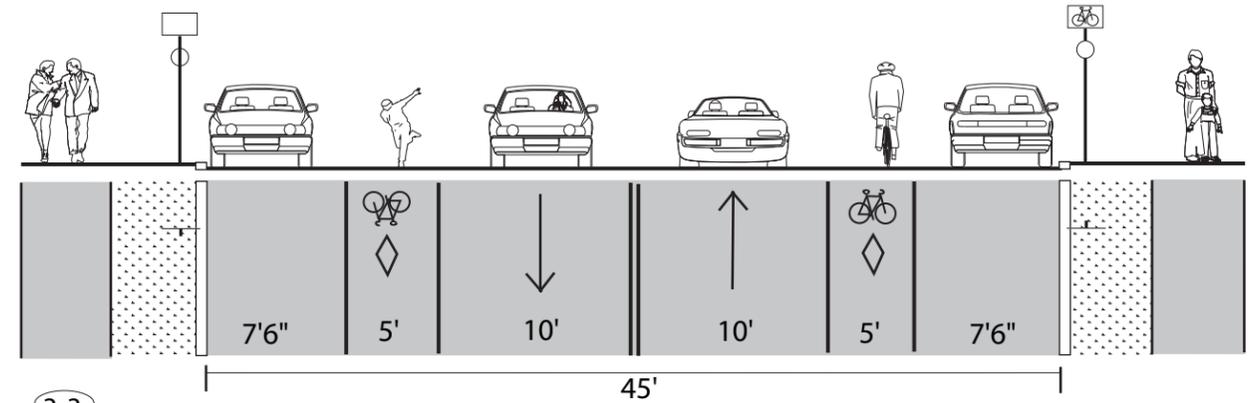


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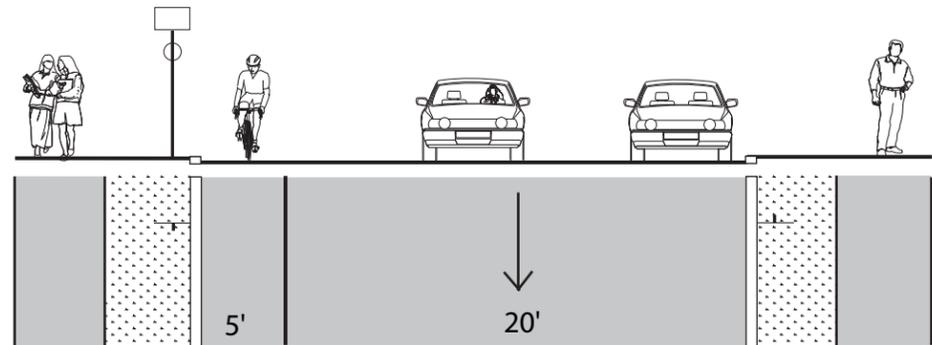


b-b

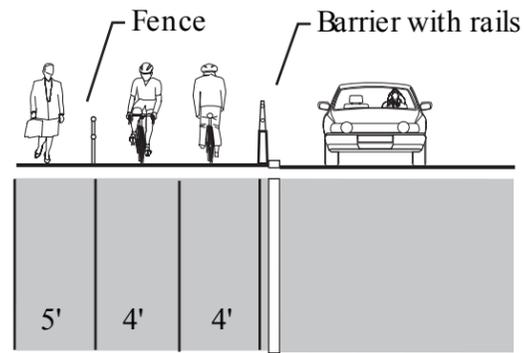
Eckford Avenue at Raleigh Street
Proposed on-street bicycle lane.



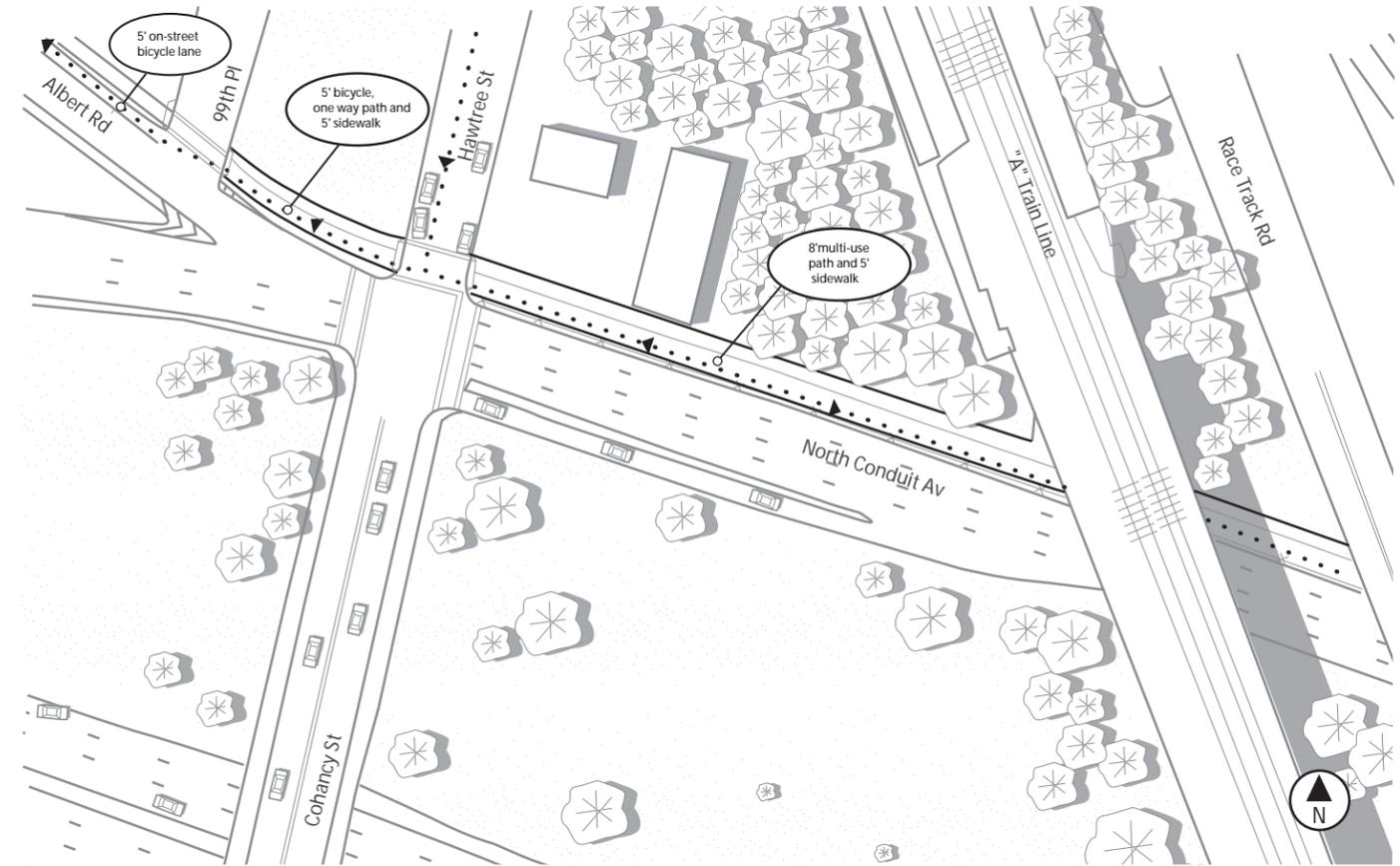
149th Avenue at 96th Street.
Proposed 5' on-street bicycle lane.



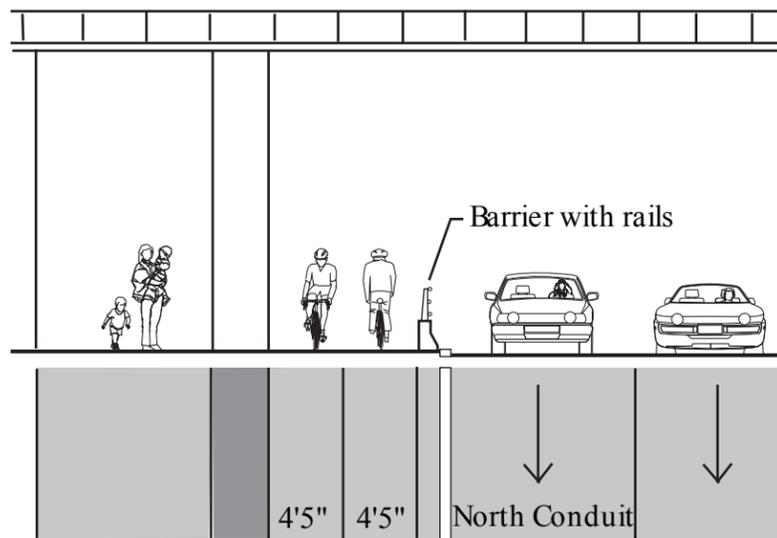
Albert Road at Raleigh Street
Proposed on-street bicycle lane.



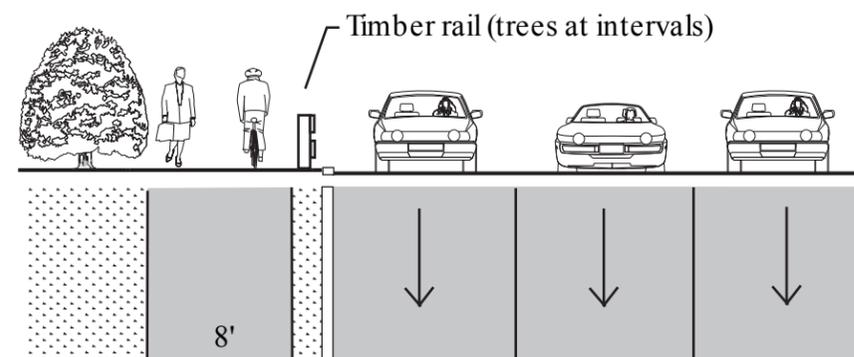
1-1 Widened concrete sidewalk along North Conduit between Cohancy Street and elevated A subway line.



B Aqueduct Subway Station



2-2 Aqueduct Subway Station, looking east. Proposed widening of concrete sidewalk by 2' and construction of railing.



3-3 Concrete sidewalk in front of Aqueduct Race Track, looking east. Proposed 3' widening of sidewalk. Users would be instructed by Greenway signage to "go slow - respect others".

Southern Queens Greenway

Segment Four: 114th Street to Van Wyck Expressway Short-Term, On-Street Route

In this portion of the Southern-Queens Greenway, the short-term, on-street route travels along quiet, tree-lined streets in South Ozone Park, a pleasant neighborhood of single-family homes. The segment begins at the intersection of 114th Street and North Conduit; a traffic light was recently installed at this intersection to facilitate pedestrian access to the ball fields at Southern Fields. The major east-

west streets selected for the Greenway, 150th and 135th, could accommodate 5' bicycle lanes, while the remaining connector streets would be equipped with Greenway signage. Segment Three's Alternative Route Two would exit the Aqueduct site and link with the Greenway at 150th Avenue and 114th Street. Officer Byrne Park, located on 135th Avenue, is a major destination in Segment Four, providing tennis

courts and ball fields. Segment Four ends at a crossing over the Van Wyck Expressway.

For further route connectivity, a unifying street planting design should be developed in concert with the community. For example, ginkgo trees might provide variation in form and vibrant fall colors.

Legend

- Proposed on-street route
- - - Route connection
- Roadway Crossing
- Site Enlargement
- Cross Section

