

Southern Queens Greenway

Segment Six: Francis Lewis Boulevard to Brookville Boulevard Long-Term, Off-Street Route

The off-street, long-term route would follow the original Southern Parkway paths, roughly between Farmers Boulevard and the Long Island Rail Road (LIRR). Within Segment Six, the paths are buffered from the heavy motor vehicle traffic on North Conduit and Southern Parkway by mature trees and bushes. Prior to construction of the paths, however, analysis is needed to traffic calm the entrance to the Parkway on-ramp at 180th Street. Late morning, off-peak sight observations at this ramp revealed an almost uninterrupted use as motorists enter from either west bound North Conduit or south bound 180th Street. Views of the entering motorists from the existing path are obstructed by mature trees and fencing. In addition, the relatively limited length of the on-ramp requires a rapid increase in speed as motorists must merge with the Parkway's high speed traffic.

One potential solution is the designation of a left-turn lane with a signalized entrance to the ramp. A thorough traffic study is needed to determine this proposal's feasibility. If this proves infeasible, another option is the installation of a new bicycle/pedestrian bridge over the on-ramp. With a path width of 10', a bridge should rise to a height of 19'; handicapped accessible ramps would travel to the west and east to allow direct east-west travel along the Greenway.

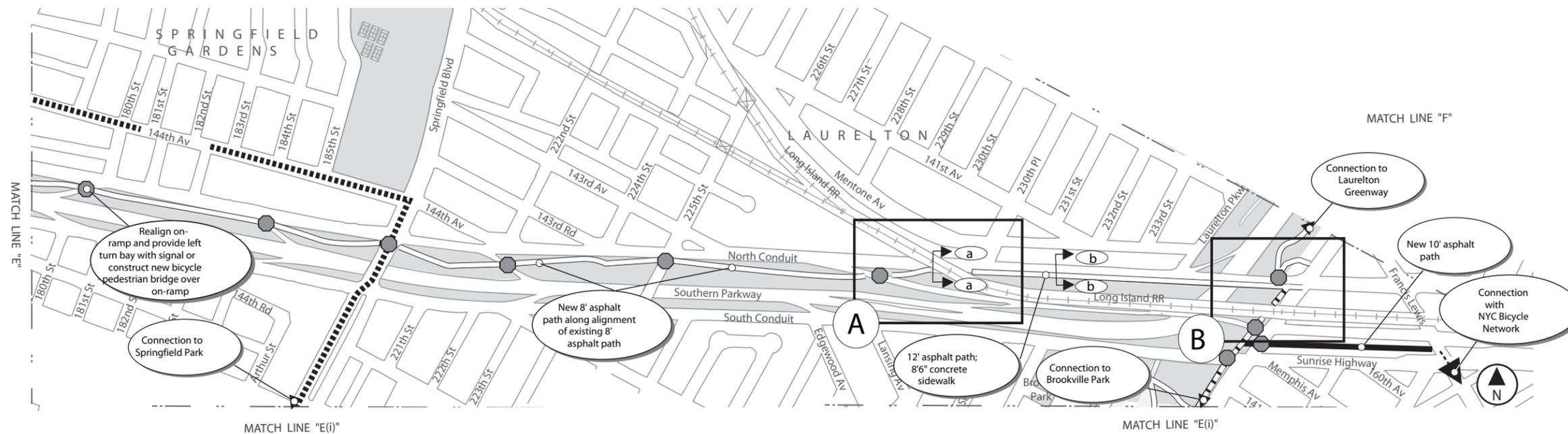
East of the on-ramp, the Greenway will travel along new 8' asphalt paths, replacing deteriorated paths. The Parkway's off-ramps will be improved with high visibility crosswalks and signage, instructing Greenway users and motorists to stop at the crosswalks. East of 224th Street, the Greenway travels underneath the LIRR bridge. Two wide travel lanes currently exist on both sides of the bridge's

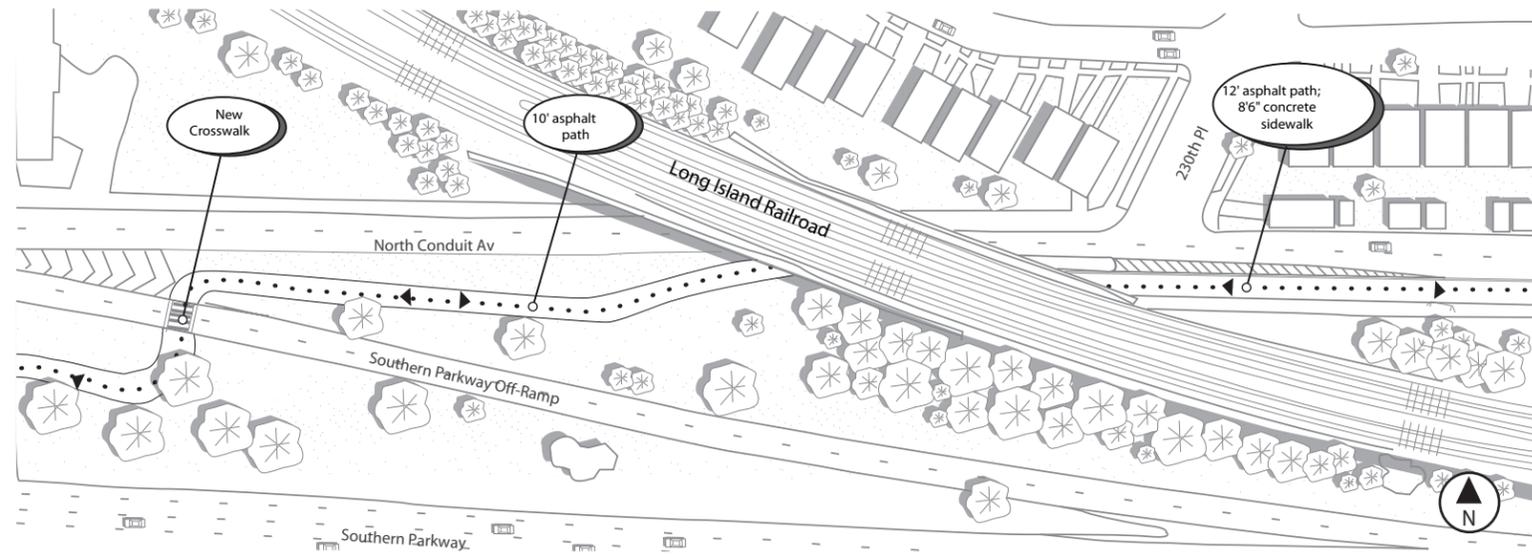
center beam. We propose reconstruction of the southern lane as the Greenway route, providing a 10' multi-use lane and an 8'6" concrete sidewalk; the northern lane would be improved with striping to provide two, 11' motor vehicle travel lanes. While a thorough traffic analysis is needed, site observations of low traffic volumes along this section of North Conduit from the LIRR bridge to Brookville Boulevard indicate that the roadway could be reduced from four 10' lanes to two 12' lanes. Within the remainder of the roadway, a 2' jersey barrier, 12' asphalt multi-use path and 7' concrete sidewalk would be constructed to provide a first class Greenway route. This proposal replaces an existing, inadequate 3' sidewalk and reduces the excessively

wide motor vehicle roadway. To the east, the Greenway passes Mentone Park, a largely unused and undeveloped park located between the LIRR and North Conduit, and connects with Brookville Boulevard. Mentone Park should be assessed for compatible development as the Greenway's design is developed in this section. At this junction, Greenway users have three travel choices: to the north, the Laurelton Greenway; to the east, a connection with the NYC bicycle network and Nassau County beaches via a new 12' asphalt path within the Sunrise Highway right-of-way open space; or to the south, a greenway loop route connecting Springfield, Idlewild and Brookville Parks.

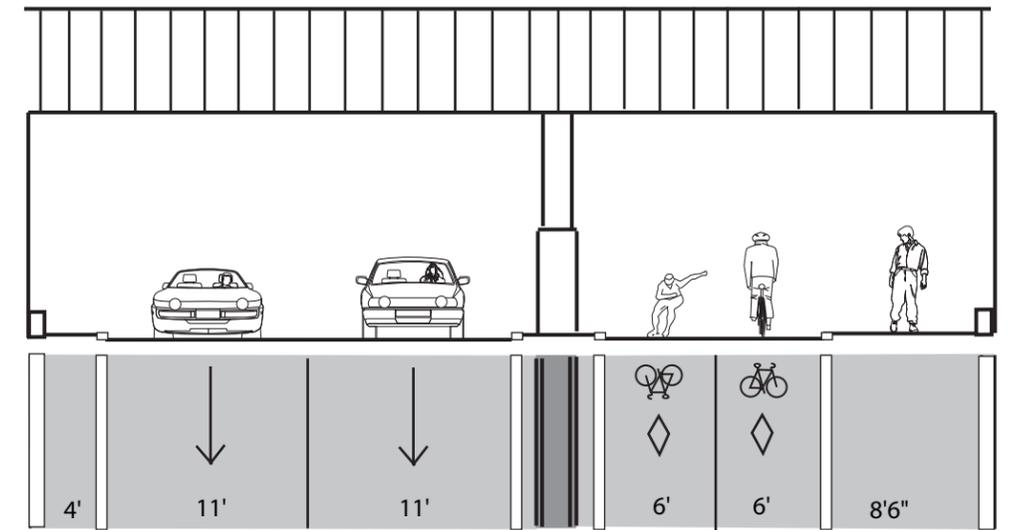


- Legend
- Proposed on-street route
 - Proposed reconstructed path
 - Proposed path
 - Route connection
 - Roadway Crossing
 - Site Enlargement
 - Cross Section

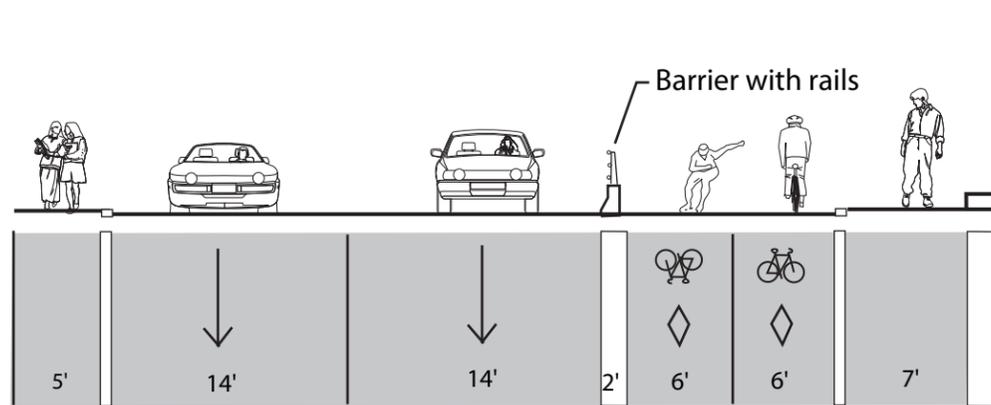




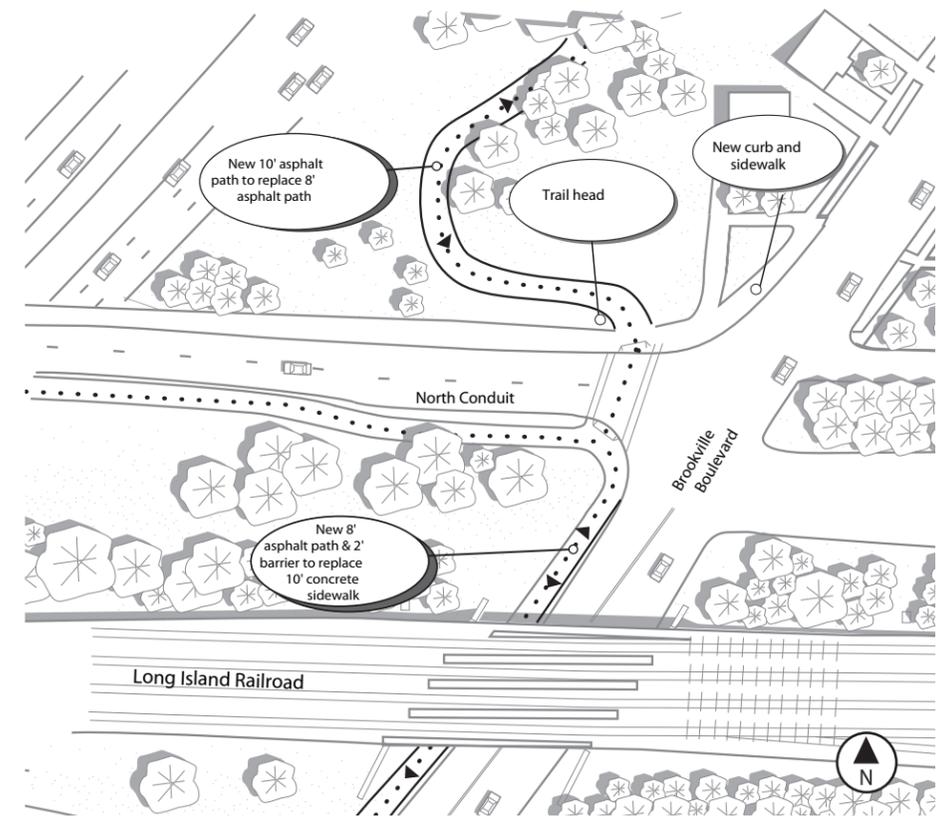
A Off-street route: North Conduit at 230th Place



a-a Off-street route: North Conduit underneath LIRR Bridge, looking east. Proposed 12' multi-use asphalt path and 8'6" concrete sidewalk to replace 20' motor vehicle lane. New lighting is required for all users.



b-b North Conduit at Mentone Park, looking east. Proposed 10' asphalt path, widened sidewalk (by 2') and 2' barrier to replace 14' motor vehicle lane.



B Off-street route: Brookville Boulevard and North Conduit

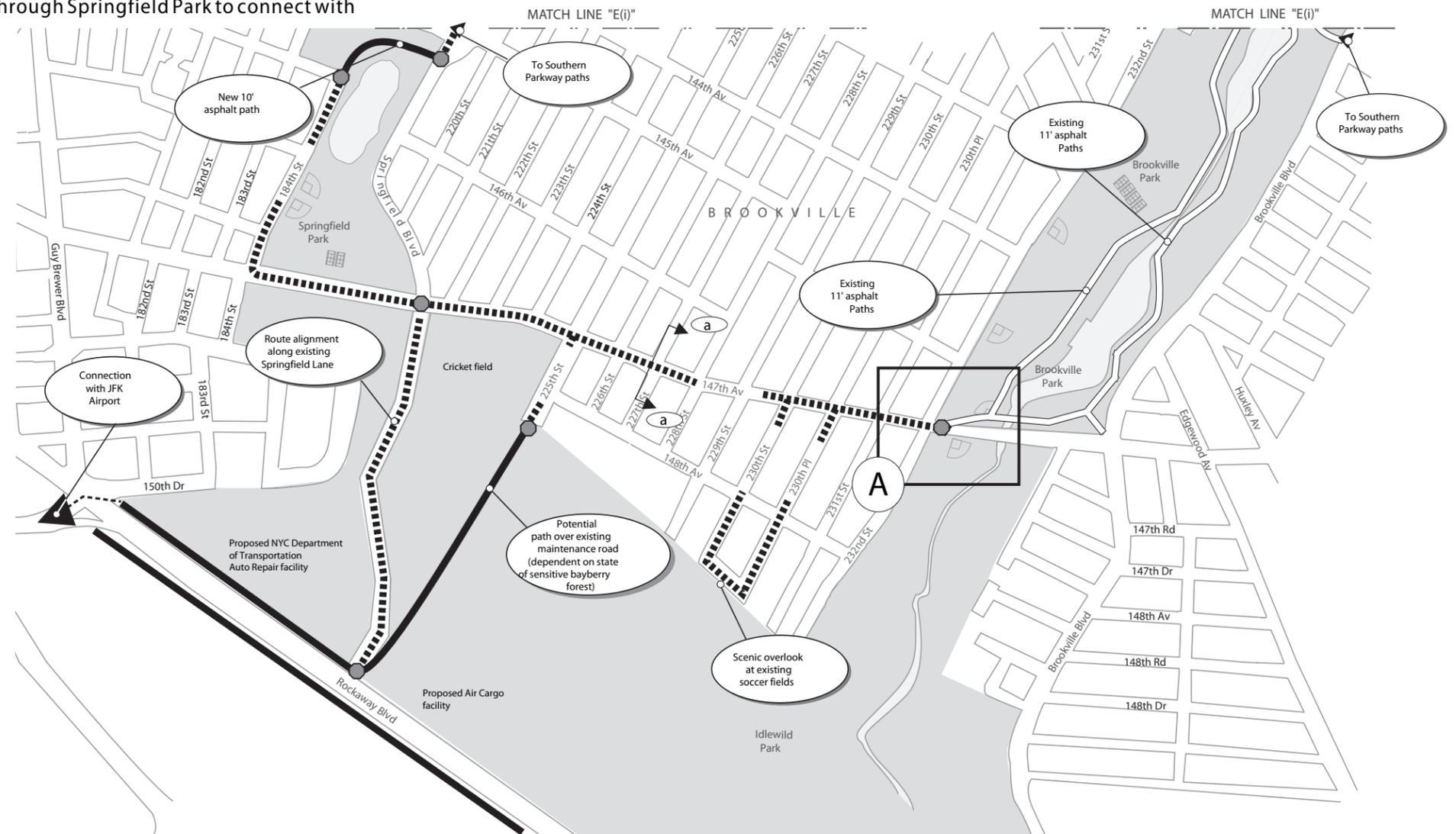
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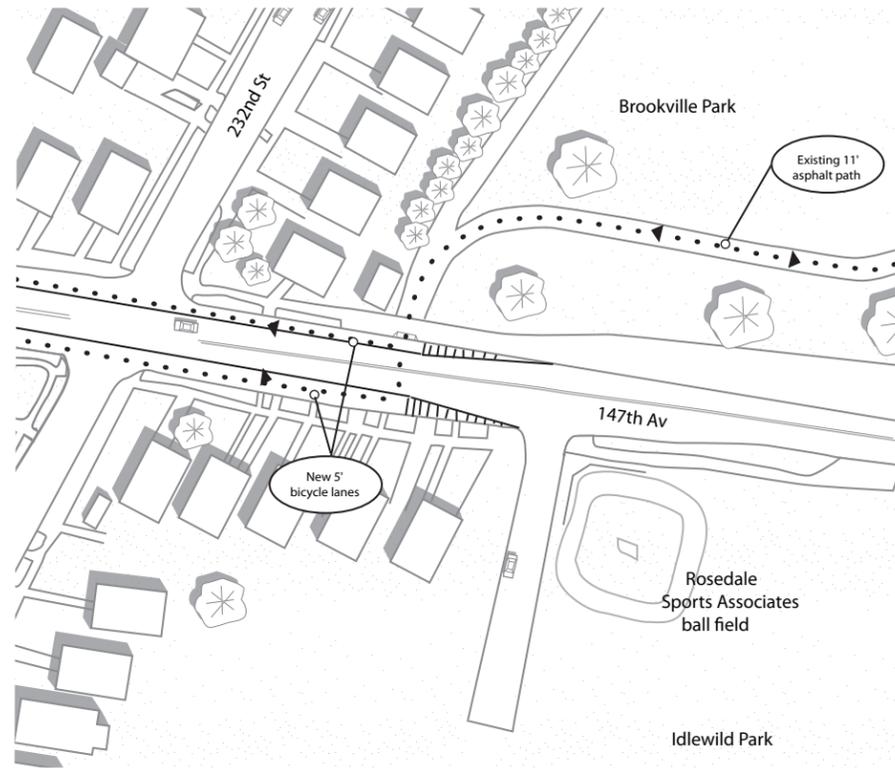
Segment Seven: Springfield - Idlewild - Brookville Park Loop

Segment Seven provides an exciting opportunity for a recreational greenway loop connecting the lakes and the paths in Brookville, Springfield and Idlewild Parks. From the intersection of North Conduit and Brookville Boulevard, the Greenway travels south on a path underneath the LIRR bridge and along Brookville Boulevard. The existing 5' concrete sidewalk would be rebuilt as an 8' asphalt path with a barrier rail. After crossing South Conduit, the Greenway continues on a new 8' path to be developed on an existing sidewalk and connects with the existing 11' bicycle paths which loop around the lakes in Brookville Park. The paths in Brookville Park would be striped and equipped with greenway signage. Exiting at the southwestern end of Brookville Park, the Greenway travels to the west along 147th Avenue to Springfield Park on proposed 5' bicycle lanes. At 230th Place, Greenway users have the option of travelling south to the soccer fields located at the edge of Idlewild park, to the south of Brookfield School. Here, a scenic overlook would be provided to view the Park's 200 acre wetlands, containing rich and diverse ecotones including phragmites, marsh, woodland/shrub, open water bayberry forest and emergent marsh/phragmites.

Continuing west, at Springfield Boulevard (currently closed to autos), Greenway users can travel south through the upland area of Idlewild Park, alongside newly constructed cricket fields, to connect with a proposed path along Rockaway Boulevard. The Rockaway path would provide access to JFK Airport and Farmers Boulevard to the west and to a potential path in Nassau County's North Woodmere Park to the east. Use of the currently closed Springfield Boulevard is dependent upon NYC DOT's access plans for the proposed auto maintenance facility on Rockway Boulevard. There is a potential alternate greenway route from 225th Street over the existing

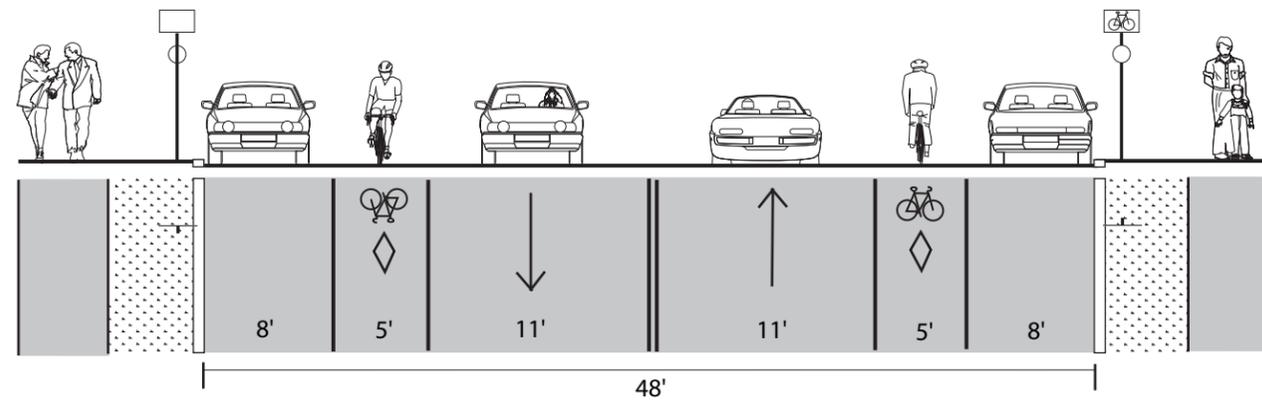
storm sewer pipe maintenance road installed by DEP. Implementation of this route, which would provide views of the wetlands to the east, would be contingent on the state of the sensitive bayberry forest planted recently along the maintenance road. To the northwest of Idlewild Park, the Greenway then travels north on 184th Street and east on a new 8' asphalt path through Springfield Park to connect with Segment Six.





Idlewild Park's new bayberry forest and maintenance road.

A 147th Avenue at Brookville Park
Proposed 5' on-street bicycle lane.



a-a 147th Avenue at 226th Street.
Proposed 5' on-street bicycle lane.